

## **Supplemental Supporting Information for a Finding of Effect**

Project: Eastport 24357.00

Scope: Sidewalk Construction

Finding of Effect: **No Adverse Effect**

---

### **Purpose and Need**

The purpose of the action is to improve safety and accessibility for pedestrians along High Street in a residential section of Eastport.

The need for this project is to address poor existing sidewalk conditions in an area that receives a high volume of vehicular and pedestrian traffic seasonally and achieve compliance with the Americans with Disabilities Act (ADA).

### **Project Background**

This project aims to address major deficiencies in the current pedestrian infrastructure at High Street in Eastport. Portions of the existing sidewalks on High Street have failed and have become obscured by encroaching vegetation. There are no ADA compliant ramps at any of the intersections and overall connectivity is inadequate.

### **Proposed Action**

The proposed action (Alternative 1) includes 5' sidewalk reconstruction on one side of High Street between Battery Street and Key Street. Between Battery Street and Third Street, the sidewalk on High Street would be placed on the west side of the roadway, and from Third Street to Key Street, the sidewalk would be on the east side. Crosswalks that would carry pedestrians across High Street would be placed at the intersections of High Street and Spruce Street, High Street and Hawkes Avenue, High Street and Third Street, High Street and Shackelford Street, and High Street and Key Street. The sidewalks would be asphalt and curbing would be concrete. The project would also include curb ramps with detectable warnings and drainage improvements. All pedestrian facilities would be ADA compliant and drainage improvements would consist of a closed storm drain system. Grass esplanades would be located between the sidewalk and roadway at Sta. 2+00L to 3+25L; 4+25L to 4+75L; 5+25L to 6+40L; 9+45R to 11+00R; 11+20R to 12+60R; 12+90R to 13+75R. The proposed action is being completed by the City of Eastport as part of MaineDOT's Locally Administered Project process.

### **Federal Action**

Federal funding

### **Definition of Area of Potential Effect (APE)**

The proposed project is located in Eastport. A map is attached below that shows the APE.

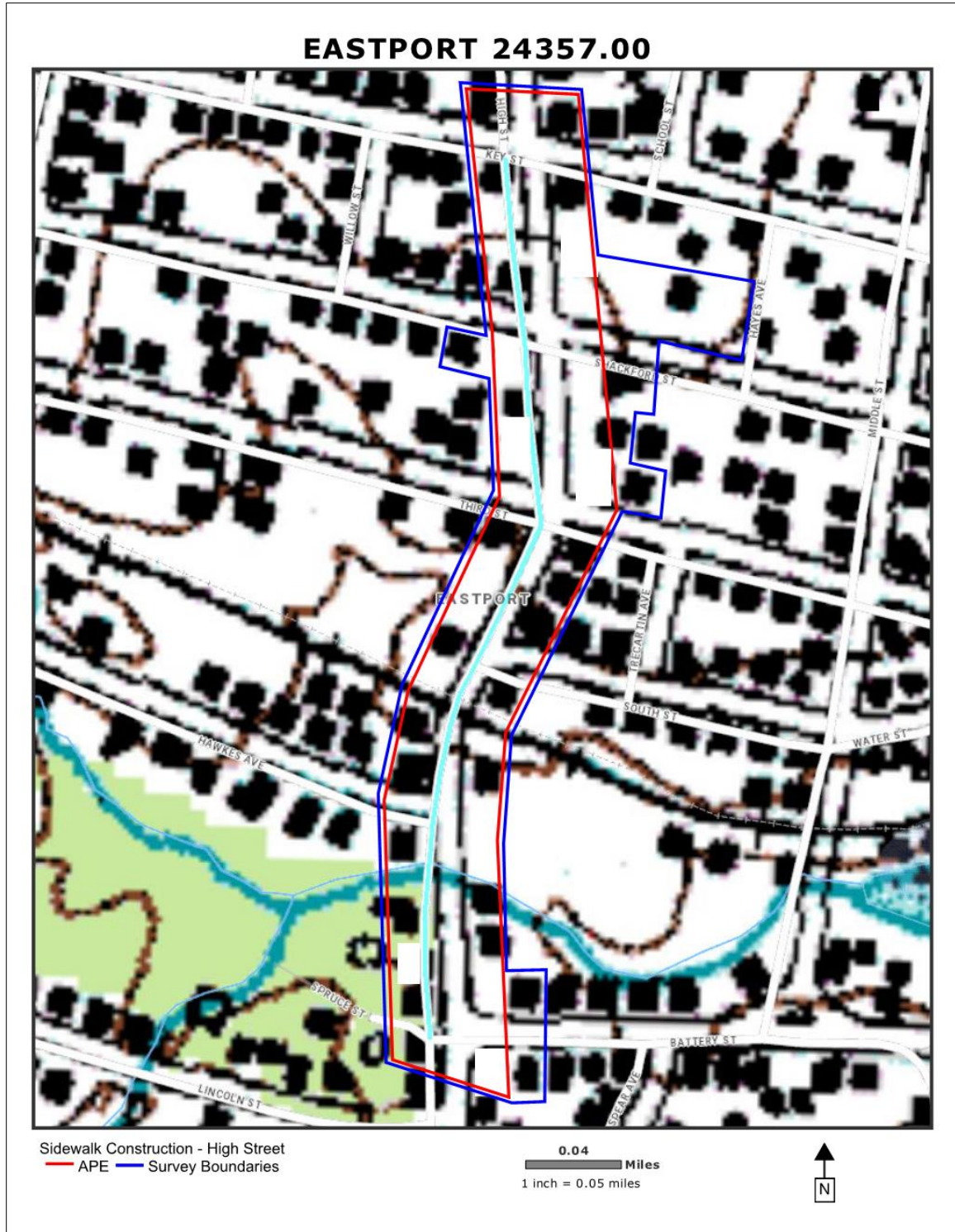


Figure 1. Eastport 24357.00 Area of Potential Effect

### Historic Properties

The proposed project is located in Eastport. The following descriptions of historic properties found within the project area are based on Maine Historic Preservation Commission (MHPC) survey forms.

#### John N. & Anna Henward House, 44-46 High Street (Allan Gore, 5+50L)



*National Register-eligible*

*Criterion C  
Architecture*

*Period of Significance  
c.1903*

The John N. and Anna Henward House is a one-and-a-half story building with a gable front roof, shed dormers, overhanging eaves, and carved fascia boards. It has a rear interior brick chimney. The house is clad in wood clapboard siding on the first story, while the upper story is clad in wood shingles with bands of wood shake shingles and fish scale shingles. The windows are two-over-one double-hung (possibly replacements) that sit in wood frames. The front façade has a bay window that consists of a large central picture window flanked by angled double-hung windows. A small diamond shaped window is located on the north elevation and is framed with small squares of glass. The wood front door is paneled with a large light in the upper half. A front porch extends from the bay window across the front of the house. It has a hipped roof, small decorative brackets below the cornice, and turned wood columns with decorative brackets. The house has a rear enclosed porch with double-hung wood windows. It sits on a mortared cut stone foundation. The contributing garage is a modest one-story building with a gabled roof, shiplap siding, and wood shingles placed in the front gabled end. The house is an excellent example of the Queen Anne style on a modest scale in Eastport. It retains all aspects of integrity. John Nelson Henwood (1871-1921) acquired this lot in 1902. The deed does not indicate that any structures were located on this property, while a 1903 mortgage deed does note buildings, which suggests that Henwood may have built the house around that time. He married Anna Bowen (1870-1946) in 1893 when he was a marble worker and she was a dressmaker. Henward remained in the marble industry, eventually becoming a retail merchant of monuments (or headstones). The house remained with Anna Henward until her death where it passed out of the family in the early 1950s.

House, 15 3<sup>rd</sup> Street (Raine Miller, 8+75R)



*National Register-eligible*

*Criterion C  
Architecture*

*Period of Significance  
c.1840*

This one-and-a-half story gable-front house demonstrates the Gothic Revival and Greek Revival styles. The front gable features decorative bargeboard, along with cornice returns and narrow cornerboards. The house is clad in wood clapboard siding and has four-over-four double-hung wood windows. A small shed roofed appendage is located on the west elevation. The front entry has pilasters and an entablature topped with a cornice. The front door may have sidelights which are covered with a moveable wooden apparatus. A metal pipe rail is located on one side of the landing at the front door. A long rear ell extends from the north elevation and has two wood windows and an entry on the west side. A brick chimney extends through the center of the rear ell. The house sits on a rusticated stone foundation. The house is an excellent example of the Gothic Revival and Greek Revival style with delicately carved bargeboard paired with a gable-front massing and classical elements. The house retains a high level of integrity and is a unique representative of these styles in Eastport. The lot may have been acquired by James Winchester, a carpenter, in 1839 and passed to his daughter Emma Young in the 1870s.



Joseph Anderson, Jr. House, 58 High Street (CEI Inc, 11+75L)



*National Register-eligible*

*Criteria B & C  
Architecture  
Politics*

*Period of Significance  
c.1850*

The Joseph Anderson, Jr. House is a two-and-a-half story side-gabled building with a center gable. It has two interior end brick chimneys and each gable peak features a carved pendant. The house is covered with asbestos siding, but wood clapboard appears to be underneath. Most windows are six-over-six wood windows with wood frames and metal storm windows. The half-story gables feature grouped windows that consist of a central two-over-two wood window flanked on each side by a shorter three light window. Two bay windows are placed along the east elevation and feature hipped roofs with scroll cut scalloped trim, four-over-four wood windows, and wood panels. A full width one-story porch is located on the north elevation with a hipped roof, square columns, and filled spandrel arches. The house also has a rear one-and-a-half story ell with a gabled dormer, rear brick chimney, two-over-two windows, and an entry. The wood paneled and glazed door is topped with a gabled hood. The house sits on a granite foundation. The Anderson House demonstrates elements of the Gothic Revival style, including the massing and roofline, along with the pendants and grouped windows. The house is an unusual and rare expression of the Gothic Revival style on a residential building in Eastport. Joseph Anderson, Jr. (1823-1896) was a master carpenter and builder in Eastport. His father was also a local carpenter. Anderson likely built this house shortly after purchasing the lot in 1849 and incorporated these popular high style elements. In addition to his trade, Anderson was served as Chair of the Eastport Board of Selectmen and moderated the town meeting for many years. His political career culminated with his election to the Maine State Legislature in 1871 and 1872 where he served in the House of Representatives. He was also a charter member of the Calais and Eastport Railroad in 1872 and presided as President of the Eastport Gas and Electric Company Board of Directors in 1895.

Archeological Resources

There are no archaeological resources in the project area.

## Impacts to Property

The following addresses potential impacts to properties as a result of the proposed action.

### John N. & Anna Henward House, 44-46 High Street (Allan Gore, 5+50L)

*National Register-eligible*

*Criterion C, Architecture*

The proposed action would result in **No Adverse Effect** to the John N. and Anna Henward House. The proposed action would include installation of 5' asphalt sidewalks, grass esplanade, and concrete curbing at this location. Asphalt sidewalk ramps would be installed on either side of the property's driveway. The driveway itself would be regraded and replaced with in-kind materials. The grass esplanade would replace an existing grass esplanade at that location. Although the replacement of the concrete sidewalk with asphalt presents a slight change to the setting, the proposed actions would not result in significant changes to the resource's aspects of integrity.

### House, 15 3<sup>rd</sup> Street (Raine Miller, 8+75R)

*National Register-eligible*

*Criterion C, Architecture*

The proposed action would result in **No Adverse Effect** to the house at 15 3<sup>rd</sup> Street. The proposed action would include the installation of 5' asphalt sidewalk, grass esplanade, and concrete curbing on the west side of the property. In addition, crosswalks would be installed at the intersection of High Street and Third Street where the sidewalk switches from the west side to the east side of the street (heading north). The crosswalks would require the installation of detectible curb ramps. The sidewalk would remain adjacent to High Street for approximately 10' before curving easterly for the esplanade. This sidewalk alignment avoids a line of mature trees associated with this resource. Any disturbed locations outside of the sidewalk or esplanade would be graded and loamed and seeded. The installation of guy wires would replace These actions would be completed on the western edge of this property and outside of the direct viewshed of the resource. The proposed action would not significantly impact the house's aspects of integrity.

### Joseph Anderson, Jr. House, 58 High Street (CEI Inc, 11+75L)

*National Register-eligible*

*Criteria B & C, Architecture, Politics*

The proposed action would result in **No Adverse Effect** to the Joseph Anderson, Jr. House. Sidewalk construction at this location would occur on the east side of High Street and not on the west side where the Anderson House is located. The proposed action would not significantly alter the resource's aspects of integrity.

### Archaeological Resources

No archaeological properties would be affected by the proposed undertaking.

## Avoidance and Minimization Efforts

The proposed action avoids significant impacts to historic resources within the project area. The use of a grass esplanade and retention of mature trees further avoids and/or minimizes

impacts to historic resources. In addition, grass esplanades would be replaced in-kind at the existing locations.

**Dismissed Alternatives**

The following alternatives were evaluated for Eastport 24357.00 sidewalk construction project.

No Build

The No Build alternative does not meet the purpose and need of the project and was therefore removed from further consideration.

**Proposed Materials**

Asphalt sidewalks, concrete curb, hot mix asphalt, grass esplanades.

**Plans**

City of Eastport, High Street Sidewalk Improvements, Washington County, MaineDOT WIN 024357.00, Draft Preliminary Design Set, April 28, 2021.

**Attachments**

J. N. Leith Smith, MHPC, to Julie Senk, MaineDOT, March 6, 2020  
Kirk F. Mohny, Concur, May 22, 2020

**STATE OF MAINE**  
**Memorandum**

Date: March 6, 2020

To: Julie Senk, Historic Coordinator, Maine DOT/ENV

From: J. N. Leith Smith, MHPC



Subject: Initial Archaeology Review

Project: WIN 24357.00, MHPC #0315-20, Sidewalk reconstruction starting at Battery Street and extending north 0.27 mile, Eastport.

Dear Julie,

After reviewing our archaeological survey records and maps, including historic maps and surficial geology maps, and comparing this information with a predictive model of archaeological site locations, we find that no archaeological fieldwork is necessary for the project listed above, based on the project location and general project description information received with your memo of February 24, 2020. It is extremely unlikely that an archaeological site would be affected by this project, in our opinion.

In following the procedures specified in the Federal Highway/MHPC/MDOT programmatic agreement, we **recommend a finding that there will be no archaeological properties affected by the proposed undertaking.**



# STATE OF MAINE

## Memorandum

Date: April 28, 2020

To: Kirk F. Mohney, MHPC  
From: Julie Senk, Maine DOT/ENV  
Subject: Section 106 request for concurrence  
Project: Eastport 24357.00, MHPC #0315-20  
Scope: Sidewalk construction



The Maine DOT has reviewed this project pursuant to the Maine Programmatic Agreement (PA) and Section 106 of the National Historic Preservation Act of 1966, as amended.

The proposed project consists of sidewalk construction on High Street beginning at the intersection with Battery Street and extending northerly for 0.27 of a mile.



In accordance with 36 CFR Part 800.4, the following identification efforts of historic properties were made:

- 800.4(a) (1) - The Area of Potential Effect (APE) includes properties/structures adjacent to the sidewalk and within the project limits. The project limits are defined by the sidewalk and the immediately adjacent area. Properties/structures adjacent to this project limit are considered to be within the APE. The APE is shown as a red polygon on the attached map.
- 800.4(a) (2) - Review of existing information consisted of researching the National Register and MHPC survey databases. The Maine Historic Preservation Commission Archaeological staff has reviewed the undertaking.
- 800.4(a) (3) - The Town of Eastport and applicable historical societies were contacted via letter and asked to comment on knowledge of, or concerns with, historic properties in the area, and any issues with the undertaking's effect on historic properties. The town was also requested to provide information regarding local historic societies or groups. No replies have been received to date.
- 800.4(a) (4) - Letters outlining project location and scope were sent to the 4 federally recognized Tribes in Maine. The Passamaquoddy Tribe and Houlton Band of Maliseets have responded with no concern about the proposed undertaking.
- 800.4(c) - The Maine DOT conducted historic architectural surveys within the APE to determine if properties met National Register criteria. Maine Historic Preservation Commission Archaeological staff has reviewed the undertaking and recommended "a finding that there will be no archaeological properties affected." **The Maine DOT has determined that four architectural properties within the APE are eligible for listing in the National Register of Historic Places.**

*In accordance with the PA and 36 CFR Part 800, please reply with your concurrence or objection to the determination of eligibility for listing in the National Register of Historic Places within 30 days. If more information is deemed necessary, please supply a list of the specific resources in question.*

Please contact me at Julie.Senk@maine.gov or 592-3486 if you have any questions. Thank you.

cc: CPD e-file  
enc: Architectural survey; J. N. Leith Smith, MHPC, to Julie Senk, MaineDOT, March 6, 2020

CONCUR	
	
Kirk F. Mohney, State Historic Preservation Officer	Date

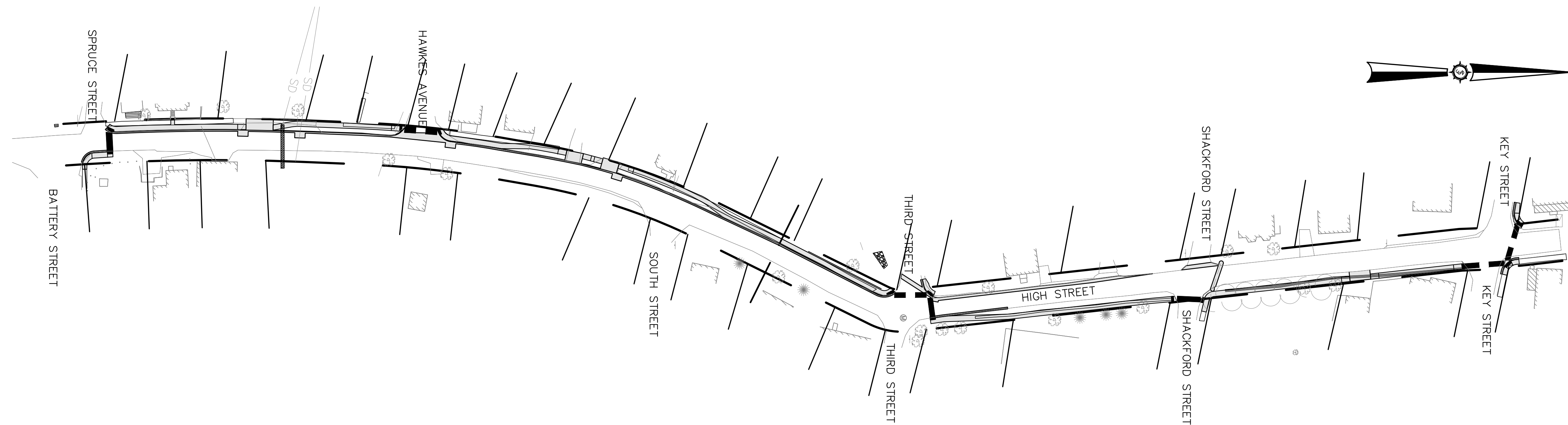
# CITY OF EASTPORT HIGH STREET SIDEWALK IMPROVEMENTS

WASHINGTON COUNTY  
FEDERAL PROJECT NO. 02435700  
MAINE DOT WIN: 024357.00  
PROJECT LENGTH: 1,425 FEET

DRAFT PRELIMINARY DESIGN SET  
APRIL 28, 2021

### INDEX OF SHEETS

Description	Sheet No.
Title Sheet	1
Typical Cross Sections	2-3
Plan & Profile	4-5
Cross Sections	6-15



PROPOSED	LINE LEGEND	EXISTING
---	PROPERTY LINE	---
---	RIGHT-OF-WAY	---
---	EASEMENT LINE	---
---	CENTERLINE	---
---	STREAM	---
---	EDGE OF PAVEMENT	---
---	CURBING	---
---	EDGE OF GRAVEL	---
---	BUILDING	---
---	DITCH LINE	---
---	GUARDRAIL	---
---	EDGE OF WETLANDS	---
---	TREELINE	---
---	SIGN	---
UP	RETAINING WALL	---
OHW	UTILITY POLE	UP
W	OVERHEAD ELECTRIC	OHW
S	WATER LINE	W
FM	SEWER LINE	S
■	SEWER FORCE MAIN	FM
■	CATCH BASIN	■
■	MANHOLE	■
■	SEWER MANHOLE	■
■	WATER SHUT OFF	■
■	WATER GATE	■
SD	HYDRANT	SD
UB	STORM DRAIN	UB
---	UNDERDRAIN	---
F	MAILBOX	F
C	FILL LINE	C
---	CUT LINE	---

TRAFFIC DATA  
SPEED LIMIT..... UNPOSTED  
CURRENT AADT (2019)..... 540

DESIGN TEAM:  
**Sewal**  
The evolution of expertise

PROJECT NUMBER: 85616E

STATE OF MAINE DEPARTMENT OF TRANSPORTATION	APPROVED	DATE
COMMISSIONER:		
CHIEF ENGINEER:		

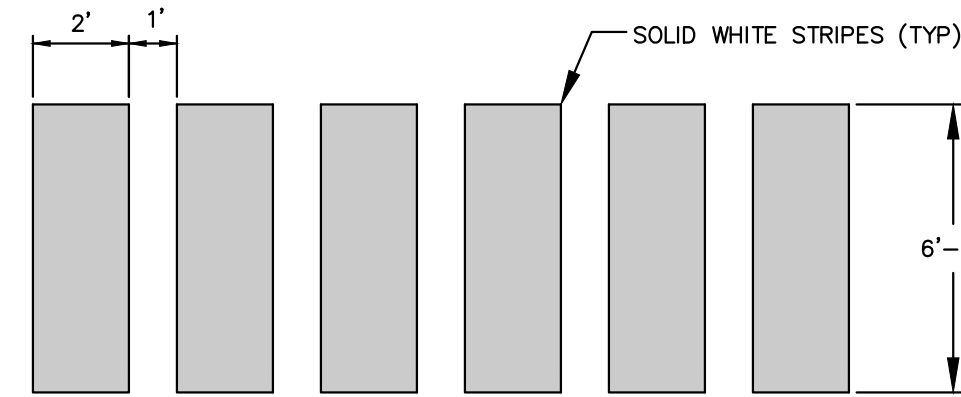
PROGRAM	MULTIMODAL
PROJECT MANAGER	
DESIGNER	
CONSULTANT	SEWAL
PROJECT RESIDENT	
CONTRACTOR	
PROJECT COMPLETION DATE	04/28/2021

CITY OF EASTPORT  
HIGH STREET SIDEWALK  
COVER

SHEET NUMBER  
1  
OF 15

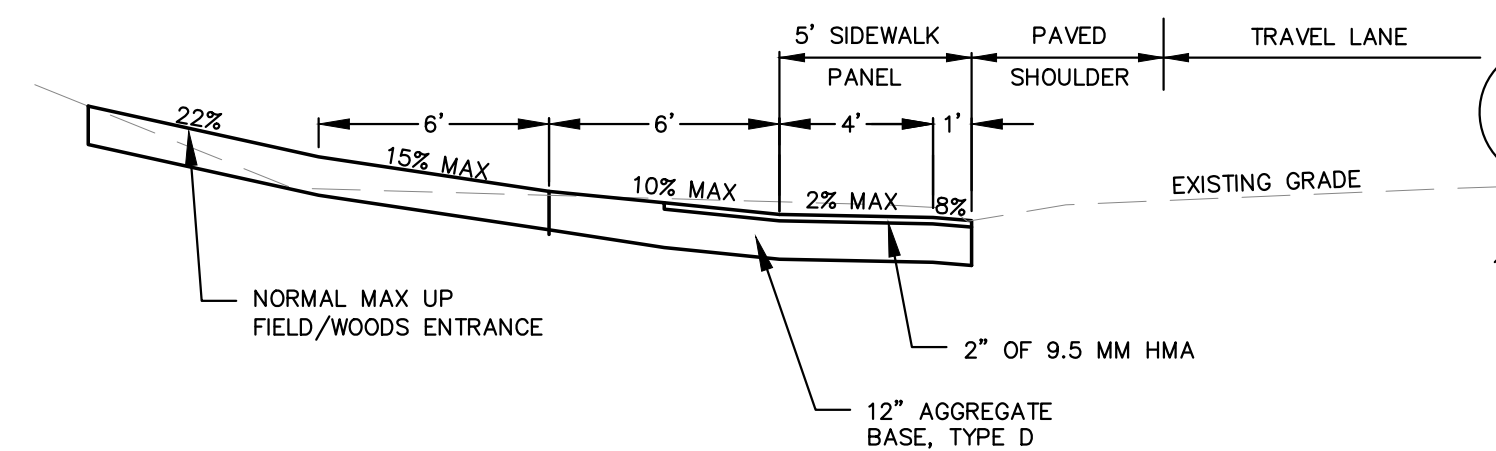
**GENERAL NOTES:**

- ALL WORK CONTEMPLATED UNDER THIS PROJECT SHALL BE GOVERNED BY AND BE IN CONFORMITY WITH STATE OF MAINE DEPARTMENT OF TRANSPORTATION "STANDARD DETAILS" (2020) AND ANY SUPPLEMENTALS THERETO, TOGETHER WITH THE "STANDARD SPECIFICATIONS" (2020) AND ANY SUPPLEMENTALS THERETO, AS MODIFIED BY THE PLANS OR SPECIFICATIONS SPECIAL PROVISIONS.
- THIS SURVEY WAS CONDUCTED ON THE GROUND USING A TOPCON DS203 ROBOTIC TOTAL STATION ON AUGUST 2, 2019. THE HORIZONTAL DATUM IS IN STATE PLANE COORDINATES, NORTH AMERICAN DATUM OF 1983-MAINE EAST. ALL UNITS SHOWN ARE U.S. SURVEY FEET. ELEVATIONS REFER TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
- THE MAINE DEPARTMENT OF TRANSPORTATION "BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL" (LATEST VERSION) SHALL APPLY TO ALL PROJECT WORK.
- THE CONTRACTOR SHALL FURNISH THE TOWN WITH A TRAFFIC CONTROL PLAN FOR THE TOWN AND DEPARTMENT TO REVIEW AND APPROVE PRIOR TO THE START OF CONSTRUCTION ACTIVITY.
- ALL PAVEMENT MARKINGS SHALL CONFORM TO THE MUTCD MANUAL, PART 8 (LATEST VERSION).
- UNDERGROUND UTILITIES ARE PRESENT WITHIN PROJECT LIMITS AND IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ALL AFFECTED UTILITY COMPANIES WITH A 72 HOUR NOTICE PRIOR TO ANY WORK BEING DONE NEAR A UTILITY. THE CONTRACTOR MUST ALSO COMPLY WITH 23 M.R.S.A. 3360-A, ENTITLED "PROTECTION OF UNDERGROUND FACILITIES", MAINE'S "DIG SAFE" STATUTE.
- CONTRACTOR SHALL RE GRADE EXISTING FEATURES TO ENSURE PROPER DRAINAGE. CONTRACTOR IS ALSO RESPONSIBLE FOR PROVIDING ADEQUATE FILL OVER NEW DRAINAGE PIPING AND PROVIDING ALL APPROPRIATE COUPLINGS AND CONNECTORS. WHERE CATCHBASINS ARE INSTALLED ADJACENT BUT NOT DIRECTLY AT EXISTING LOW POINTS, CONTRACTOR SHALL MAKE ADJUSTMENTS TO ENSURE PROPER DRAINAGE TO NEW STRUCTURE. REQUIRED PAVEMENT AND AGGREGATES WILL BE PAID FOR AT ESTABLISHED UNIT PRICE BUT ALL OTHER WORK, SUCH AS GRINDING, SHALL BE INCIDENTAL TO CATCHBASIN INSTALLATION.
- ALL STATIONING FOR TYPICALS ARE APPROXIMATE.
- ALL CURB TERMINAL ENDS AT DRIVEWAYS SHALL BE 8' MINIMUM, 15' MAXIMUM.
- CONTRACTOR TO PROVIDE 4" OF LOAM AND SEED TO ALL DISTURBED AREAS NOT BEING PAVED.
- ALL DRIVEWAYS TO BE PAVED SHALL BE CONSTRUCTED WITH 2" OF HMA OVER 12" TYPE D GRAVEL.
- SEE PLANS FOR DRAINAGE NOTES AND CURB AND SIDEWALK LOCATIONS.
- CONTRACTOR IS TO PERFORM TEST PITS PRIOR TO ORDERING ANY STRUCTURES. NOTIFY DESIGN ENGINEER IF ANY UTILITY CONFLICTS ARE FOUND. WORK SHALL BE INCIDENTAL TO CONTRACT.
- CONTRACTOR SHALL PROVIDE 2" RIGID POLYSTYRENE INSULATION IN AREAS BETWEEN INSTALLED DRAINAGE STRUCTURES AND WATER MAIN WITH LESS THAN 2 FOOT SEPARATION.
- CONTRACTOR IS RESPONSIBLE TO PROPERLY RELOCATE ALL SIGNS IMPACTED BY THE PROJECT.
- ALL EXISTING UTILITIES EFFECTED BY CONSTRUCTION SHALL BE ADJUSTED TO FINISH GRADE INCLUDING BUT NOT LIMITED TO WATER VALVES, ETC. WORK SHALL BE INCIDENTAL TO CONTRACT.
- CONTRACTOR TO PROVIDE DETECTABLE WARNING AT ALL CROSSWALKS LOCATIONS.
- THE CONTRACTOR SHALL PROVIDE NEW CAST IRON DETECTABLE WARNING PLATES AS MANUFACTURED BY ONE OF THE MANUFACTURERS LISTED ON MAINE DOT QUALIFIED PRODUCTS LIST OF CAST IRON DETECTABLE WARNING PLATES. THIS LIST CAN BE FOUND AT: [HTTP://WWW.MAINE.GOV/MDOT/TRANSPORTATION-RESEARCH/QPL.PHP](http://www.maine.gov/mdot/transportation-research/qpl.php)
- PORTLAND CEMENT CONCRETE USED FOR NEW CAST IRON DETECTABLE WARNING PLATE SHALL MEET THE REQUIREMENTS OF SECTION 502 (MAINE DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS) STRUCTURAL CONCRETE, CLASS A.
- PRIOR TO STARTING ALL WORK, THE CONTRACTOR SHALL SUBMIT FOR APPROVAL THE NAME OF THE SELECTED SUPPLIERS, MANUFACTURER'S LITERATURE DESCRIBING THE PRODUCT, INSTALLATION PROCEDURES, AND ROUTINE MAINTENANCE REQUIRED.



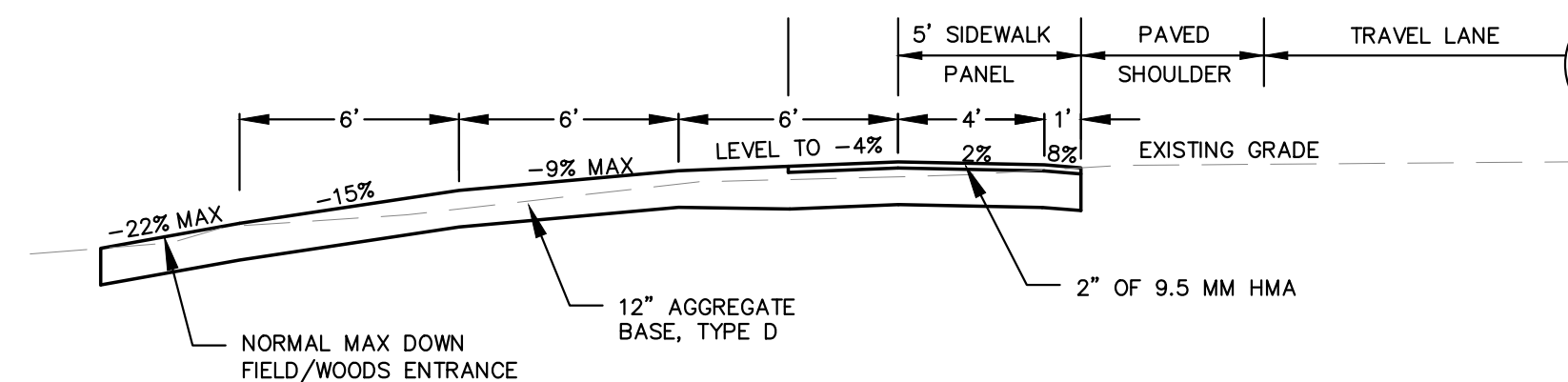
NOTE:  
CROSSWALK PAVEMENT MARKING SHALL CONFORM TO  
"MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES -  
LATEST EDITION"

**CROSSWALK PAVEMENT MARKING**  
NOT TO SCALE



NOTE:  
PAVEMENT WILL BE PROVIDED WHERE  
PAVED DRIVEWAYS CURRENTLY EXIST.

**TYPICAL DRIVEWAY SECTION IN CUT**



NOTE:  
PAVEMENT WILL BE PROVIDED WHERE  
PAVED DRIVEWAYS CURRENTLY EXIST.

**TYPICAL DRIVEWAY SECTION IN FILL**

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

WIN  
024357.00  
HIGHWAY PLANS

PROJ. MANAGER	DATE	BY	SIGNATURE	P.E. NUMBER	DATE
DESIGN-DETAILED					
CHECKED-REVIEWED					
DESIGN2-DETAILED2					
DESIGN3-DETAILED3					
REVISIONS 1					
REVISIONS 2					
REVISIONS 3					
REVISIONS 4					
FIELD CHANGES					

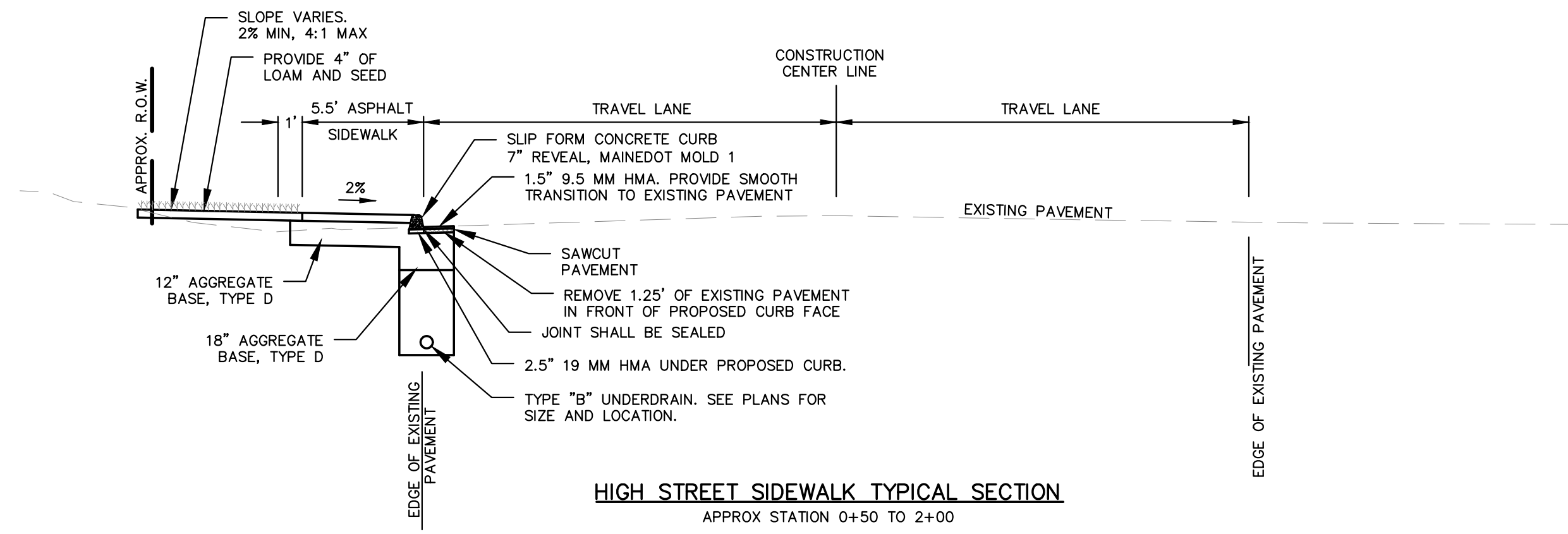
CITY OF EASTPORT  
HIGH STREET SIDEWALK

TYPICAL DETAILS

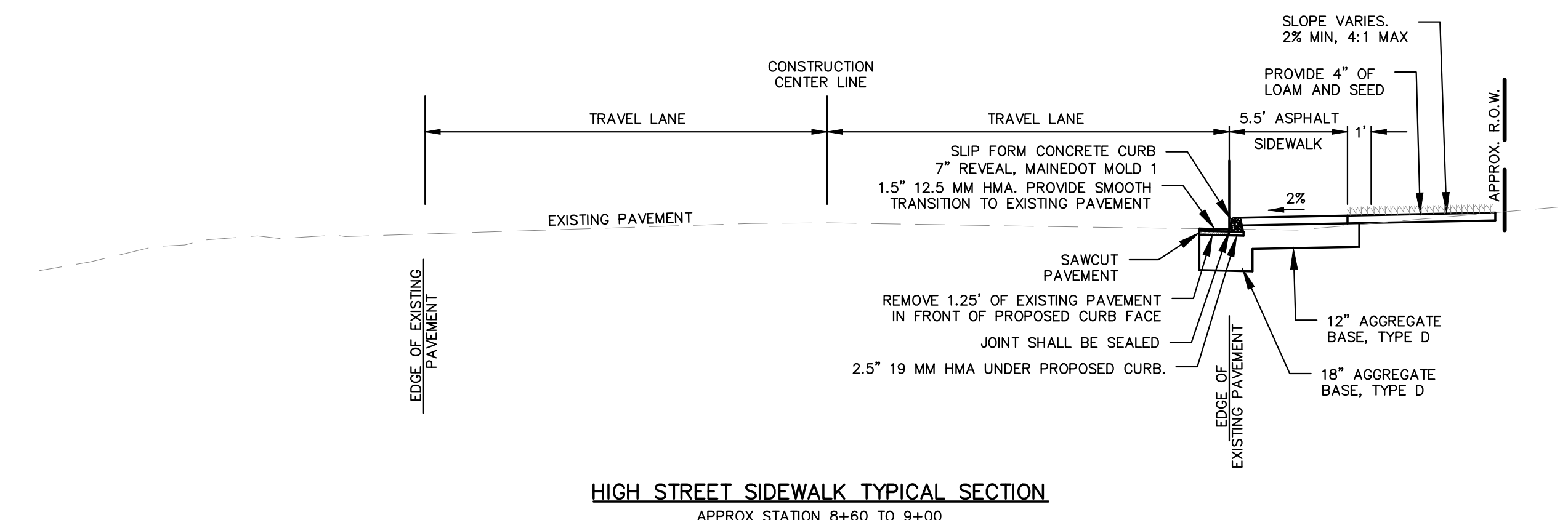
SHEET NUMBER

2

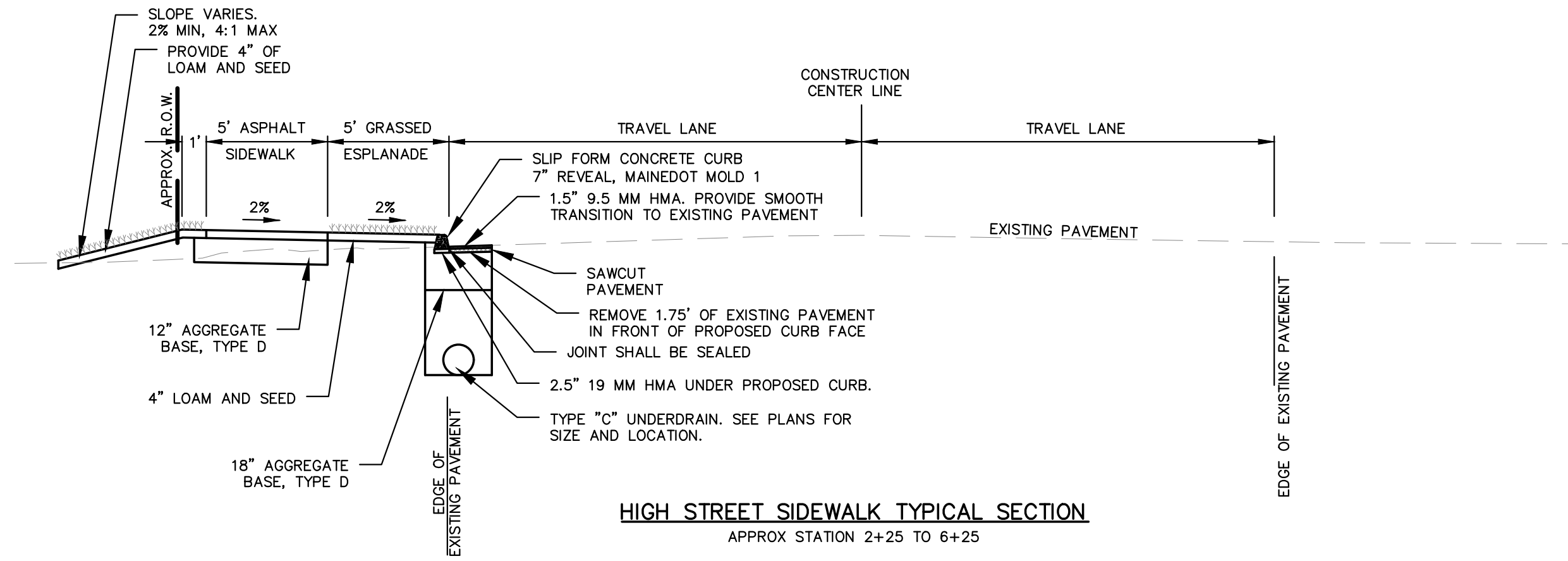
OF 15



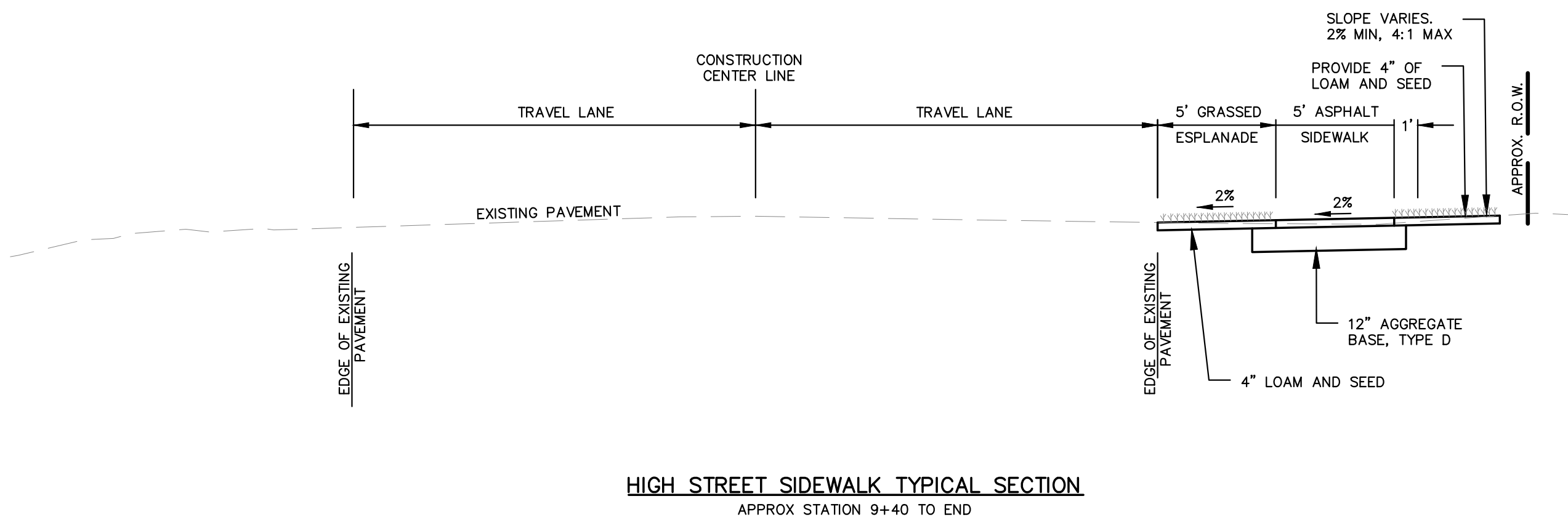
**HIGH STREET SIDEWALK TYPICAL SECTION**  
APPROX STATION 0+50 TO 2+00



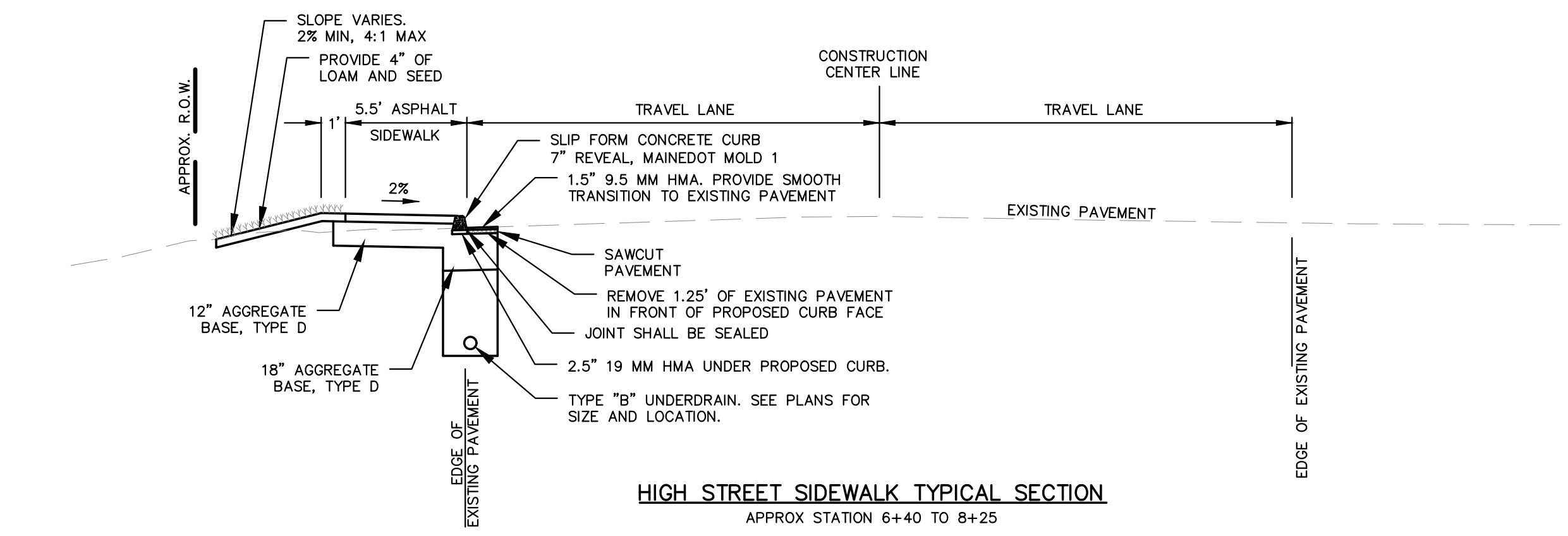
**HIGH STREET SIDEWALK TYPICAL SECTION**  
APPROX STATION 8+60 TO 9+00



**HIGH STREET SIDEWALK TYPICAL SECTION**  
APPROX STATION 2+25 TO 6+25



**HIGH STREET SIDEWALK TYPICAL SECTION**  
APPROX STATION 9+40 TO END



**HIGH STREET SIDEWALK TYPICAL SECTION**  
APPROX STATION 6+40 TO 8+25

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

WIN  
024357.00  
HIGHWAY PLANS

PROJ. MANAGER	DATE	BY	DATE
DESIGN-DETAILED			
CHECKED-REVIEWED			SIGNATURE
DESIGN-DETAILED2			
DESIGN-DETAILED3			
REVISIONS 1			P.E. NUMBER
REVISIONS 2			
REVISIONS 3			DATE
REVISIONS 4			03/31/2021
FIELD CHANGES			

CITY OF EASTPORT  
HIGH STREET SIDEWALK

TYPICAL DETAILS

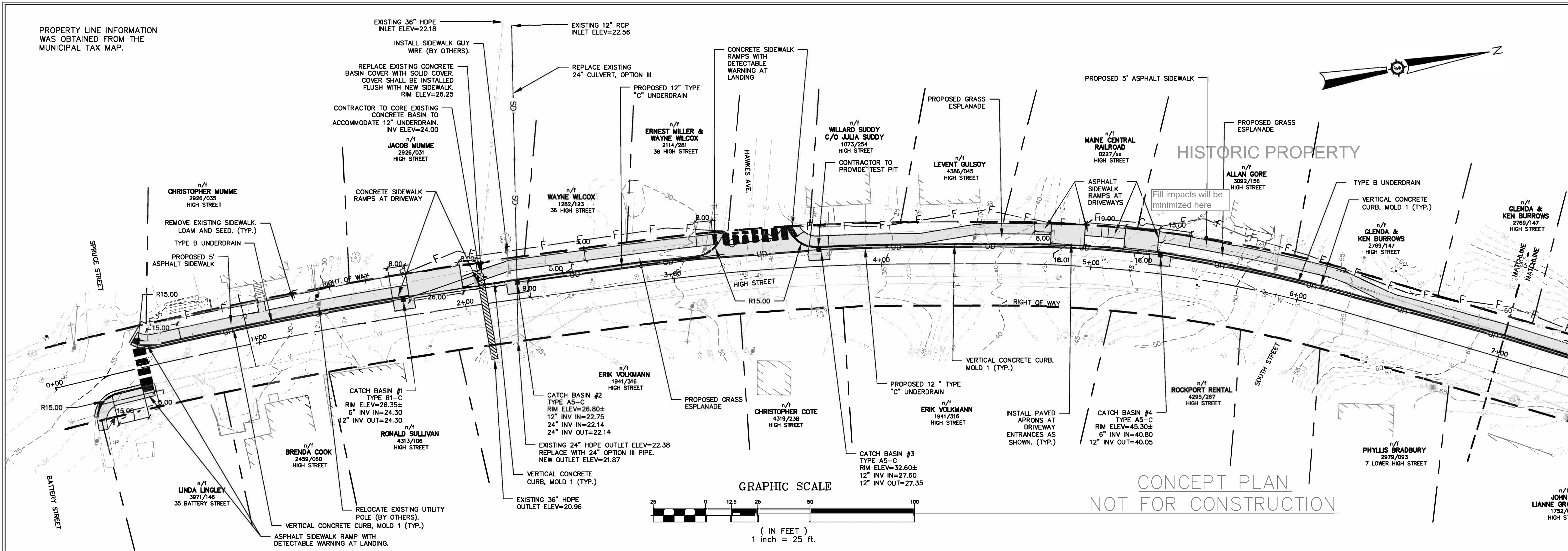
SHEET NUMBER

3

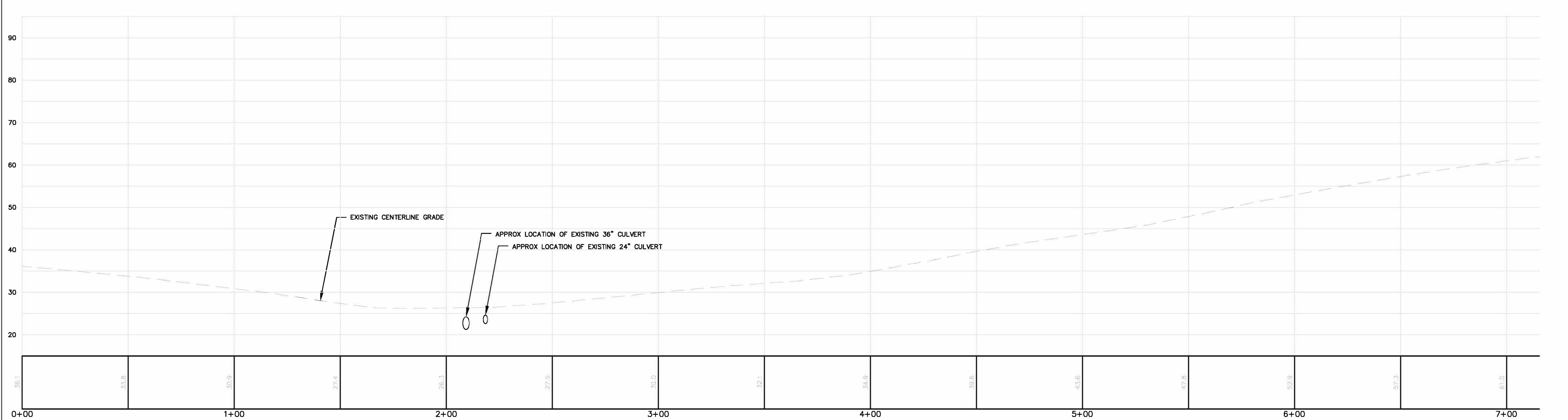
OF 15



PROPERTY LINE INFORMATION  
WAS OBTAINED FROM THE  
MUNICIPAL TAX MAP.



**HIGH STREET**  
STATION 0+00 TO 7+00



STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
WIN 024357.00  
HIGHWAY PLANS

PROJ. MANAGER	DATE	SIGNATURE	P.E. NUMBER	DATE
DESIGN-DETAILED				
CHECKED-REVIEWED				
DESIGN2-DETAILED2				
DESIGN3-DETAILED3				
REVISIONS 1				
REVISIONS 2				
REVISIONS 3				
REVISIONS 4				
FIELD CHANGES				

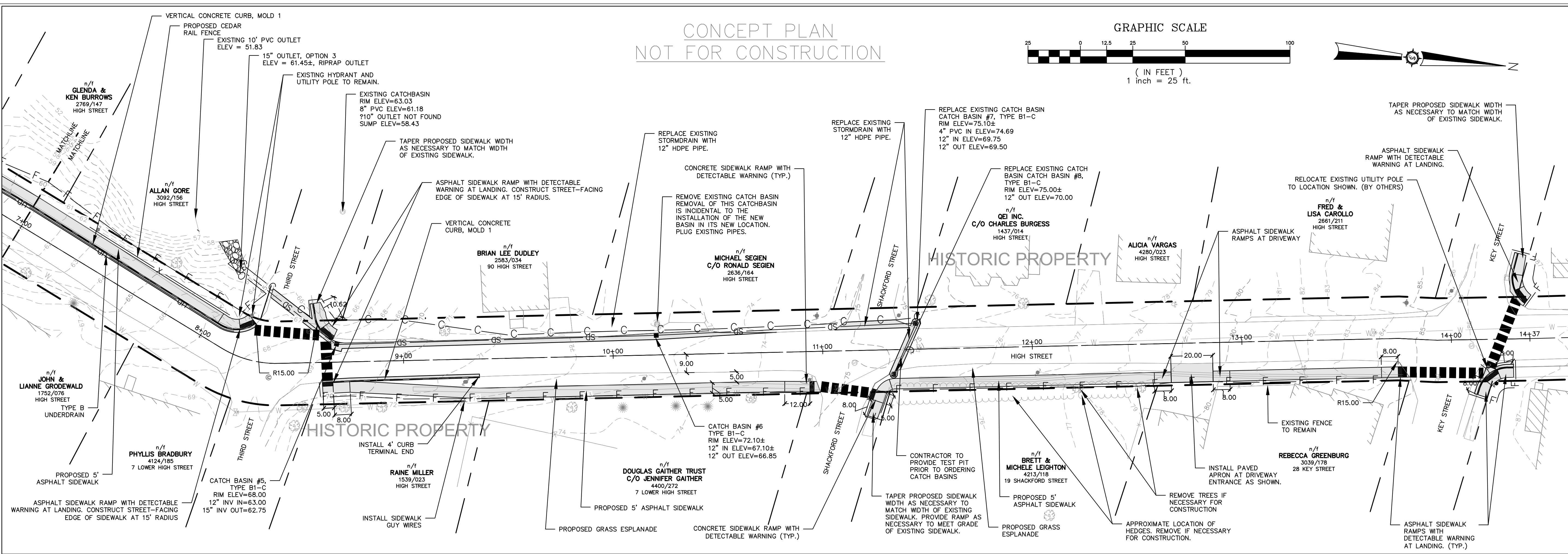
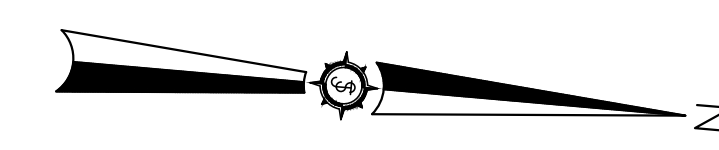
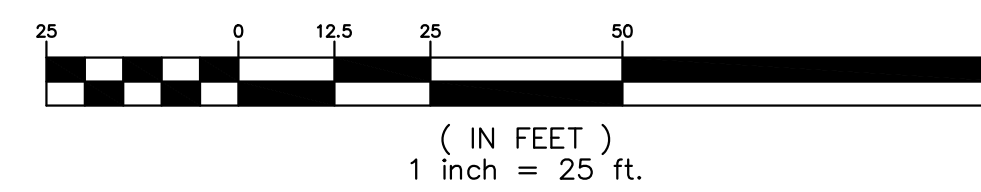
CITY OF EASTPORT  
HIGH STREET SIDEWALK

SHEET NUMBER  
4  
OF 15

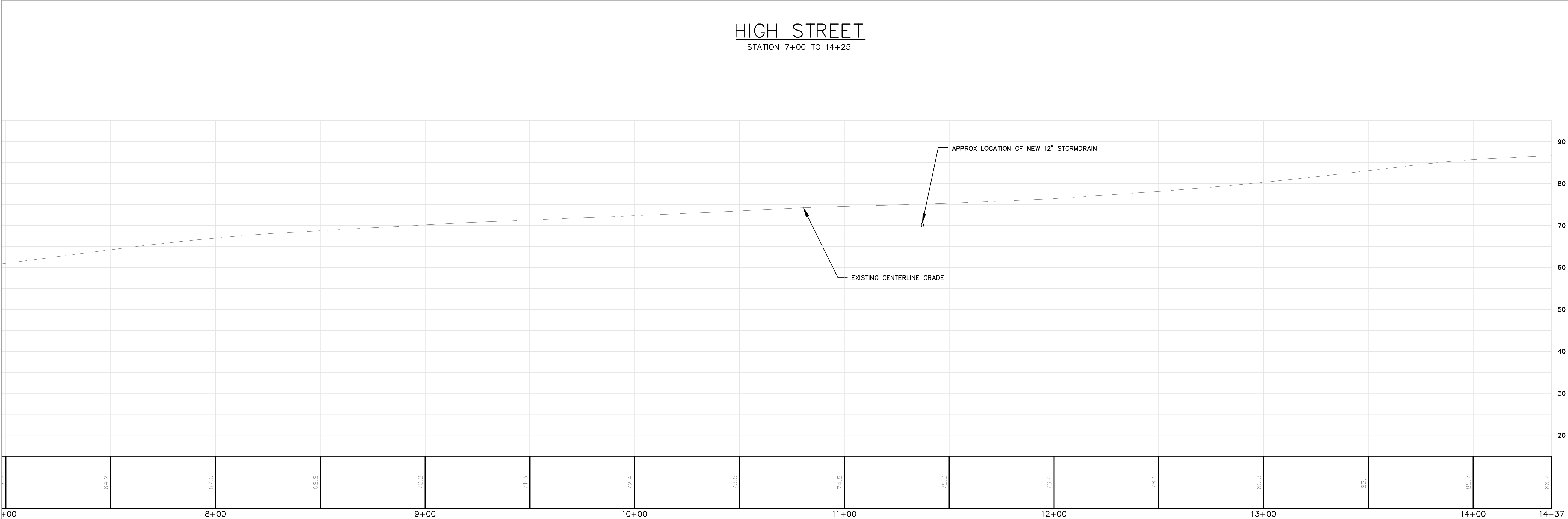


CONCEPT PLAN  
NOT FOR CONSTRUCTION

GRAPHIC SCALE



HIGH STREET  
STATION 7+00 TO 14+25



64.2	67.0	68.8	70.2	71.3	72.4	73.5	74.5	75.3	76.4	78.1	80.3	83.1	85.7	86.7
+00	8+00	9+00	10+00	11+00	12+00	13+00	14+00	14+37						

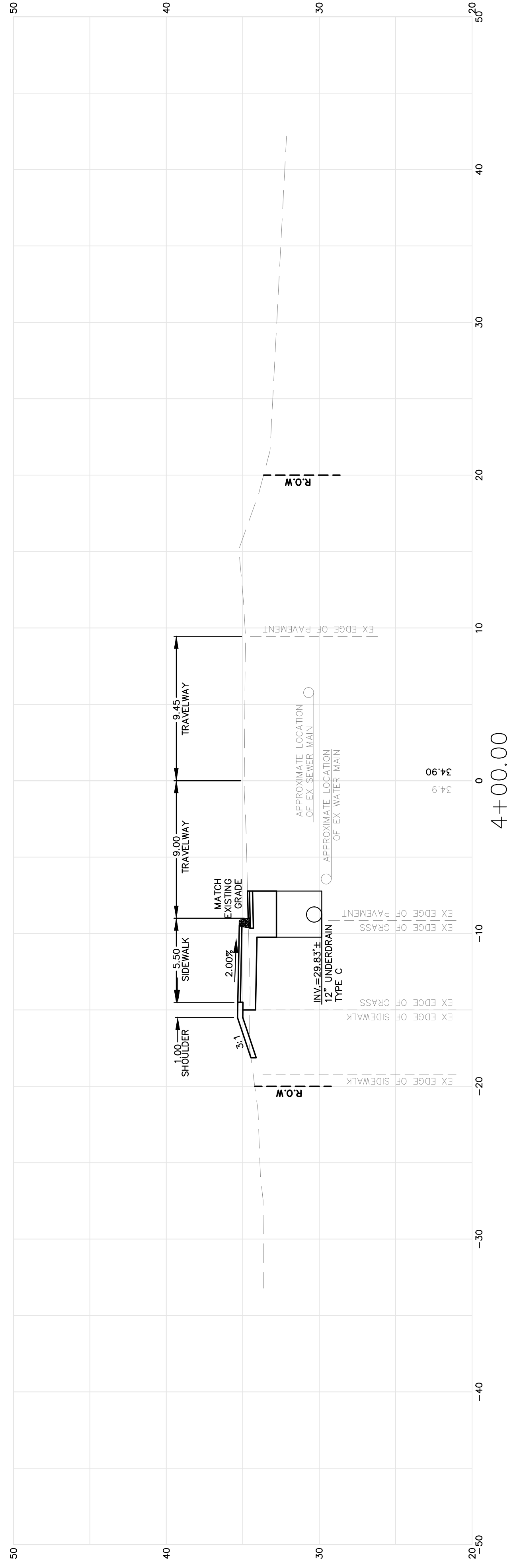
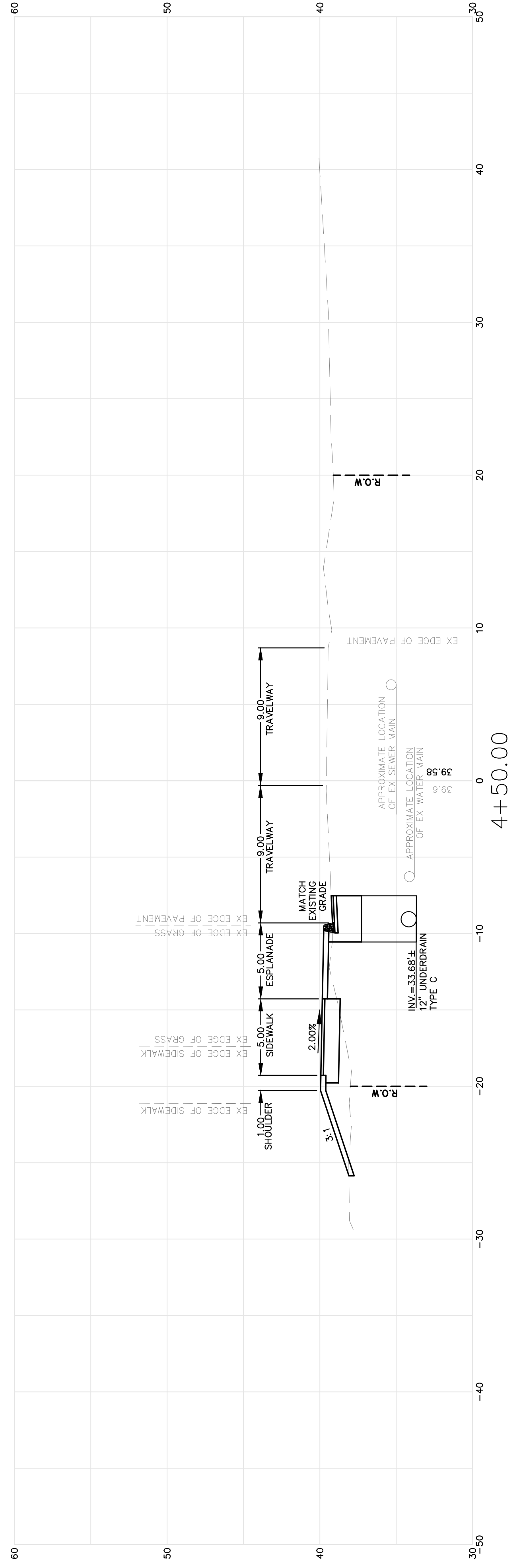
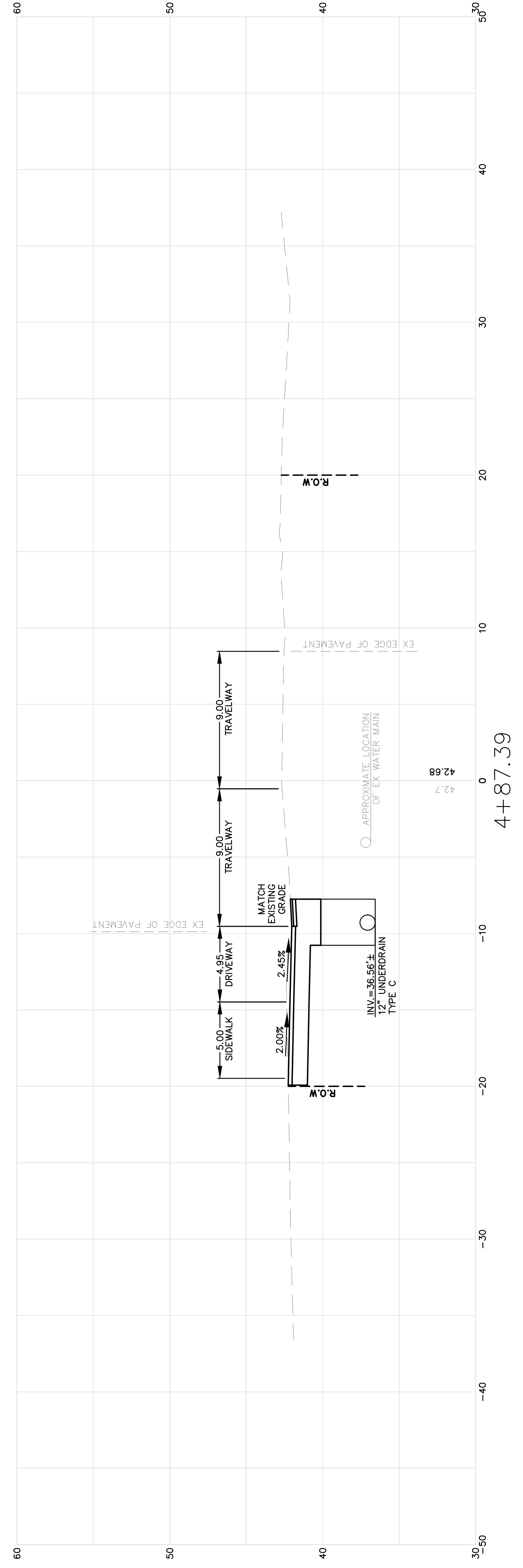
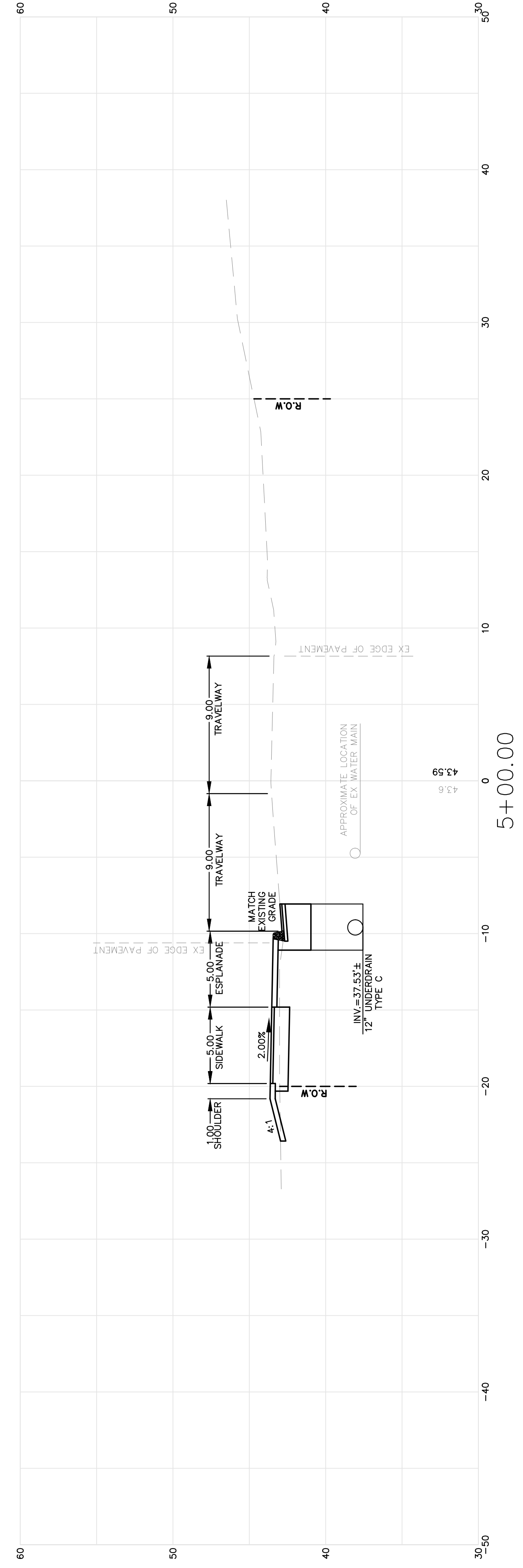
STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

HIGHWAY PLANS  
WIN 024357.00

PROJ. MANAGER	DATE	BY	SIGNATURE	P.E. NUMBER	DATE
DESIGN-DETAILED					
CHECKED-REVIEWED					
DESIGN2-DETAILED2					
DESIGN3-DETAILED3					
REVISIONS 1					
REVISIONS 2					
REVISIONS 3					
REVISIONS 4					
FIELD CHANGES					

CITY OF EASTPORT  
HIGH STREET SIDEWALK

SHEET NUMBER  
5  
OF 15



OF 15

CITY OF EASTPORT  
HIGH STREET SIDEWALK  
4+00.00 CROSS SECTIONS 5+00.00

PROJ. MANAGER	BY	DATE
DESIGN-DETAILED	-	-
CHECKED-REVIEWED	-	-
DESIGN-DETAILED	-	-
DESIGN-DETAILED	-	-
REVISIONS 1	-	-
REVISIONS 2	-	-
REVISIONS 3	-	-
REVISIONS 4	-	-
FIELD CHANGES	-	-

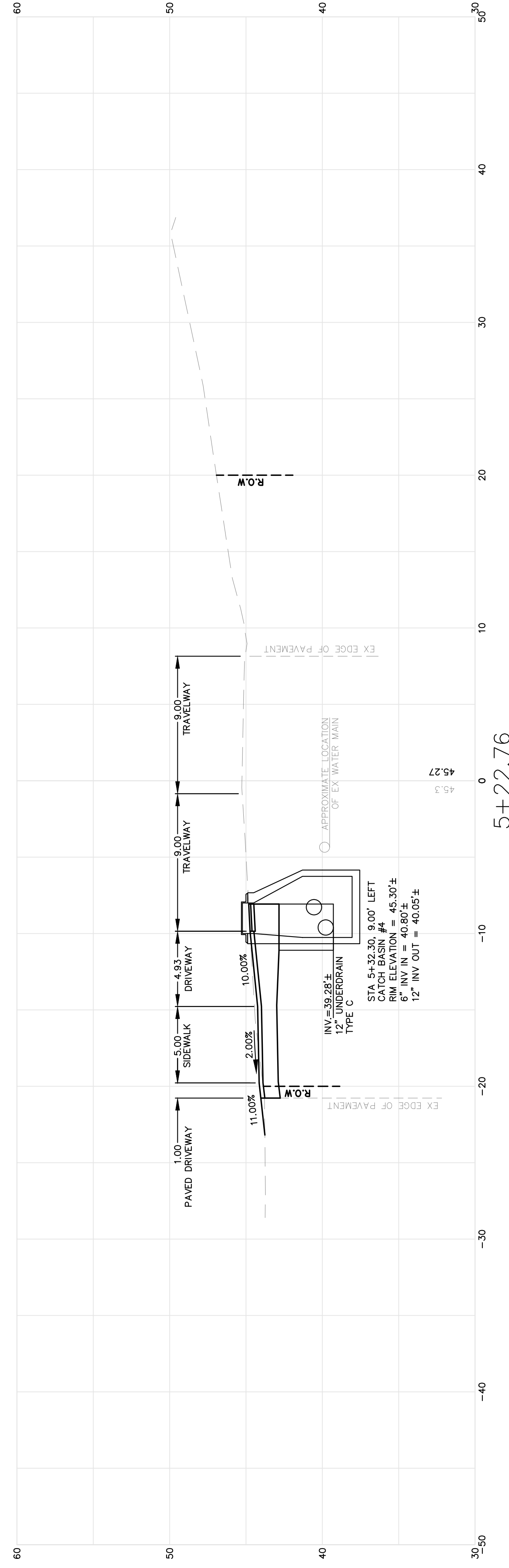
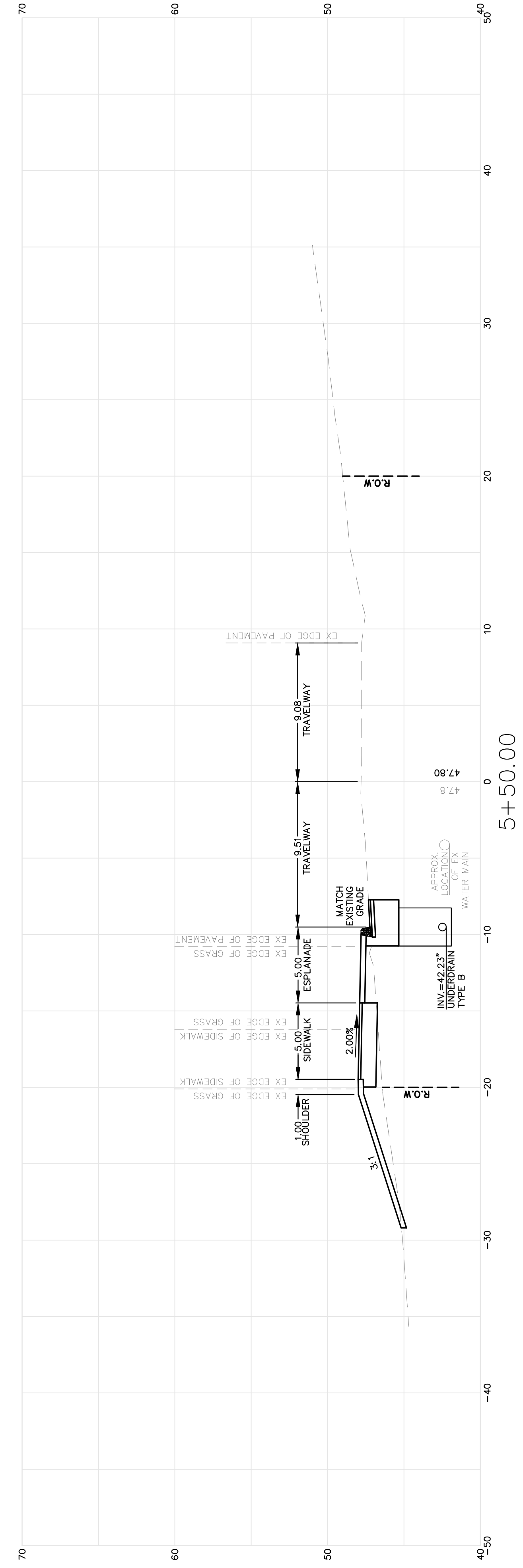
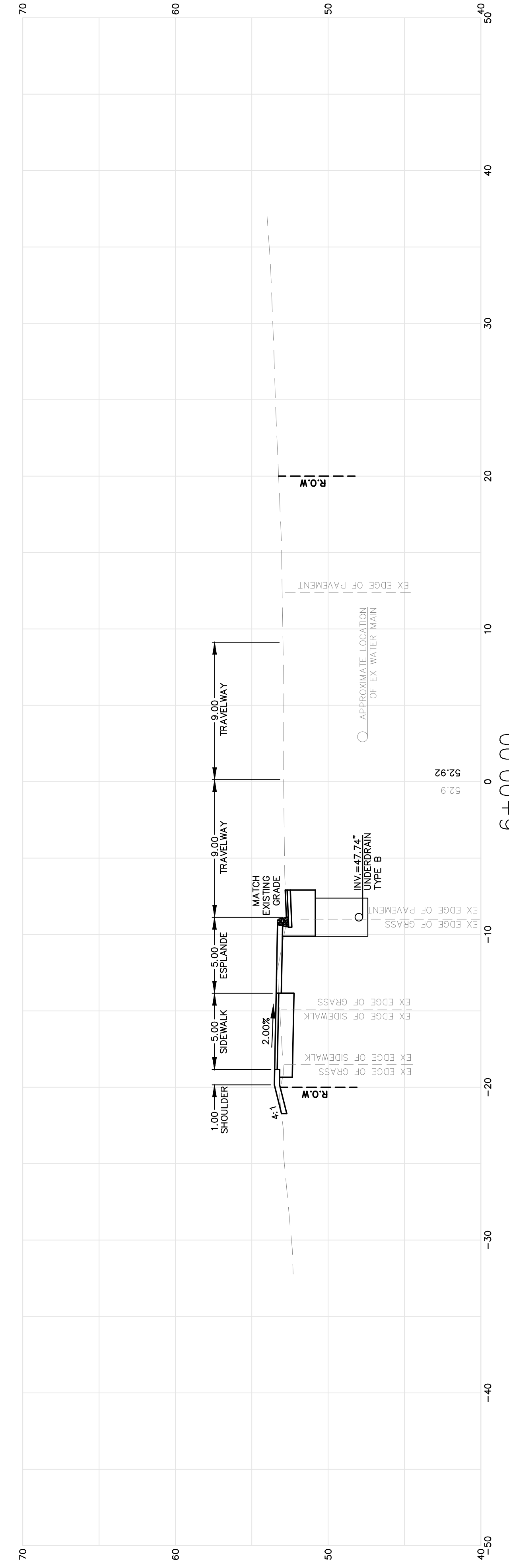
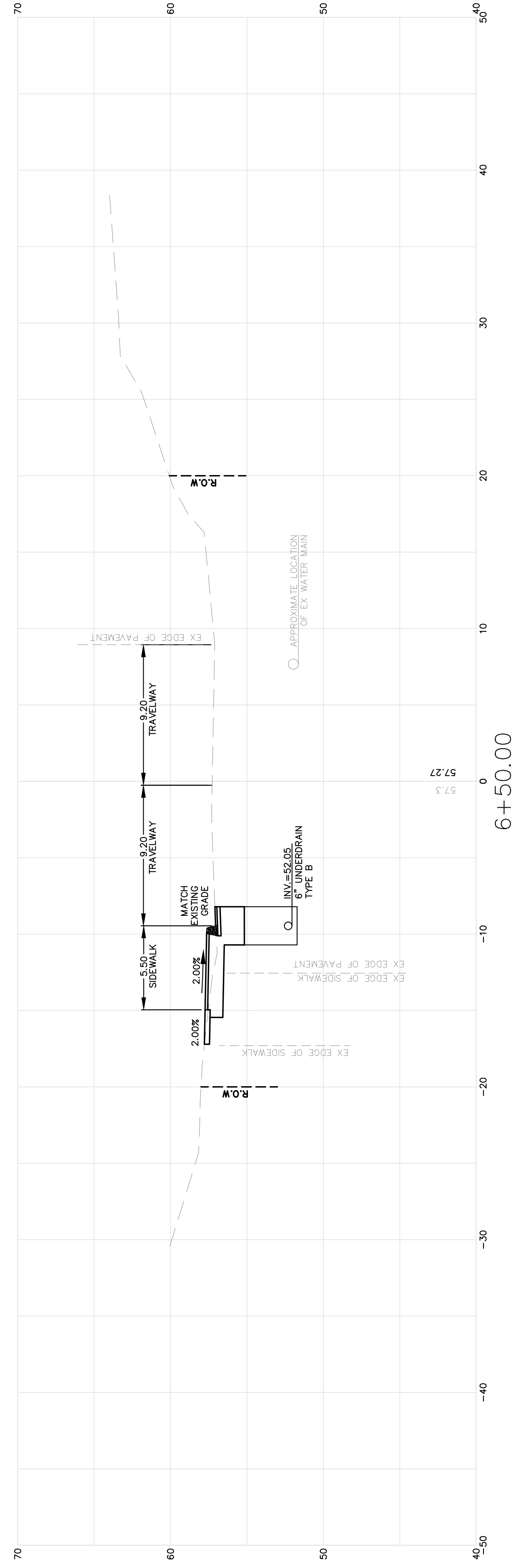
SHEET NUMBER  
0

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

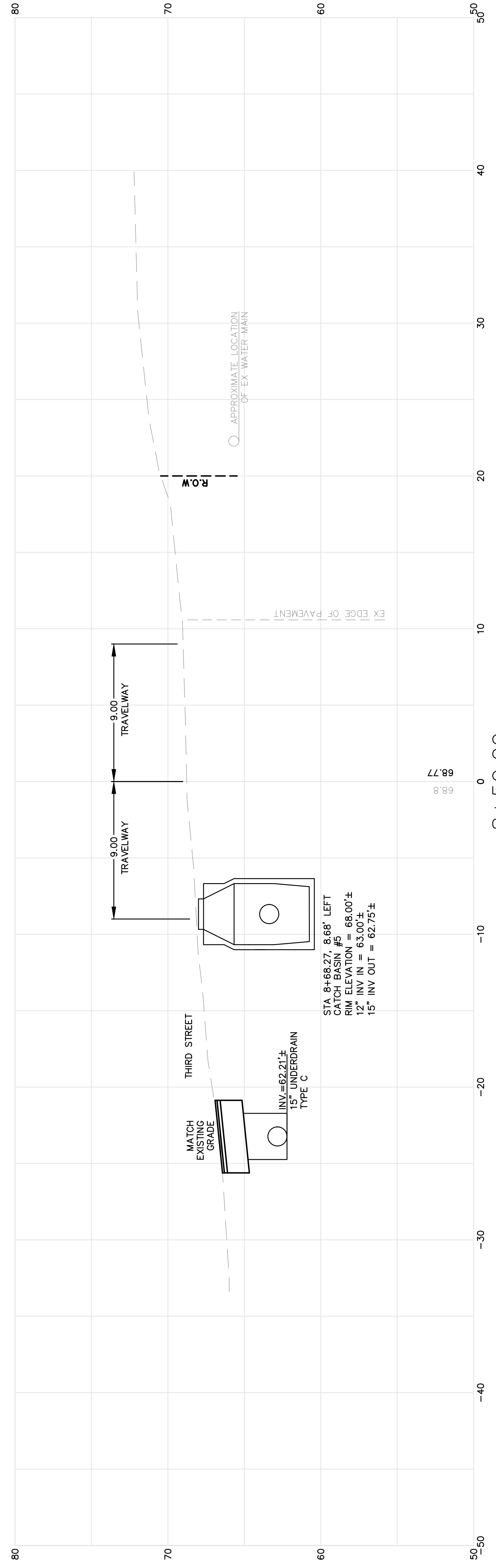
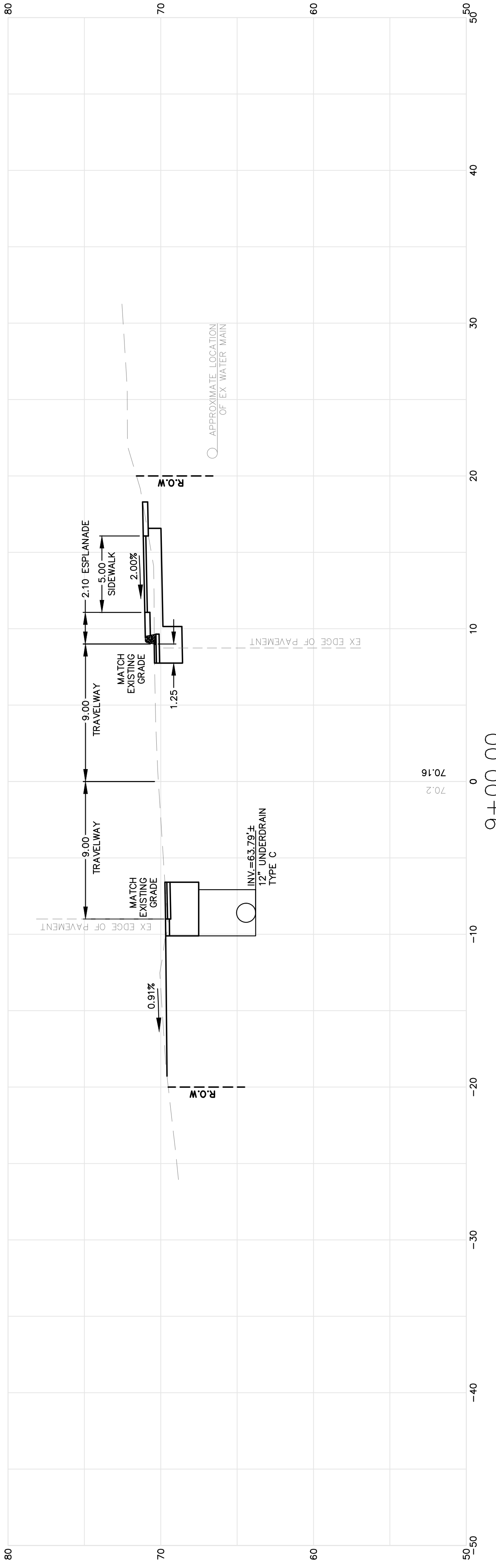
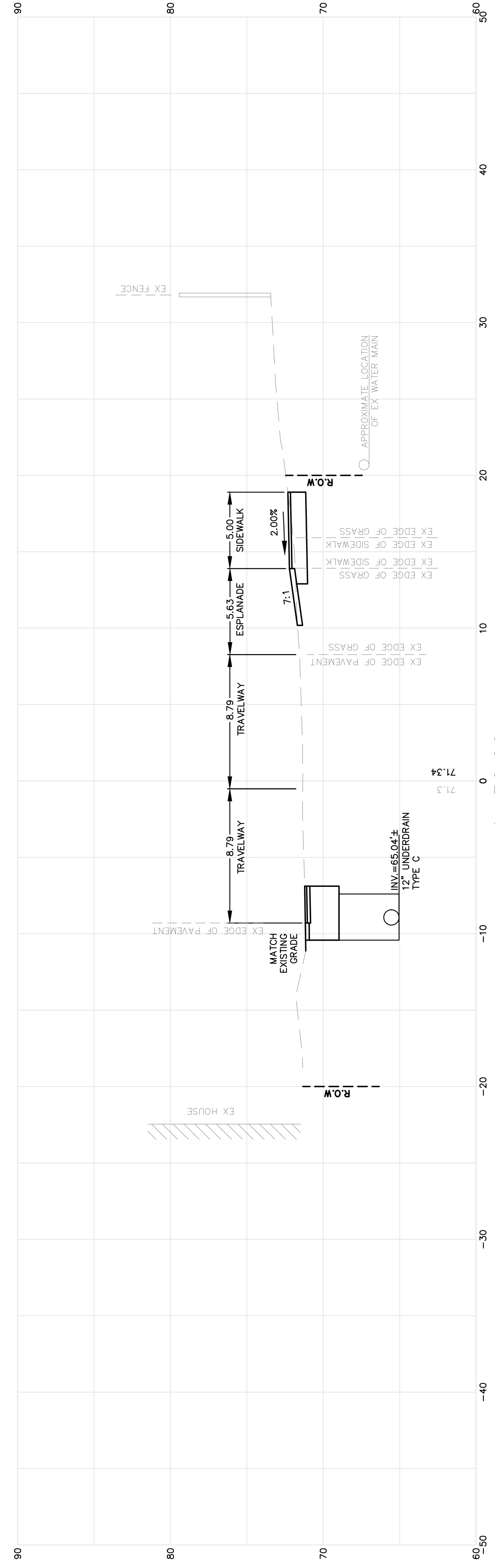
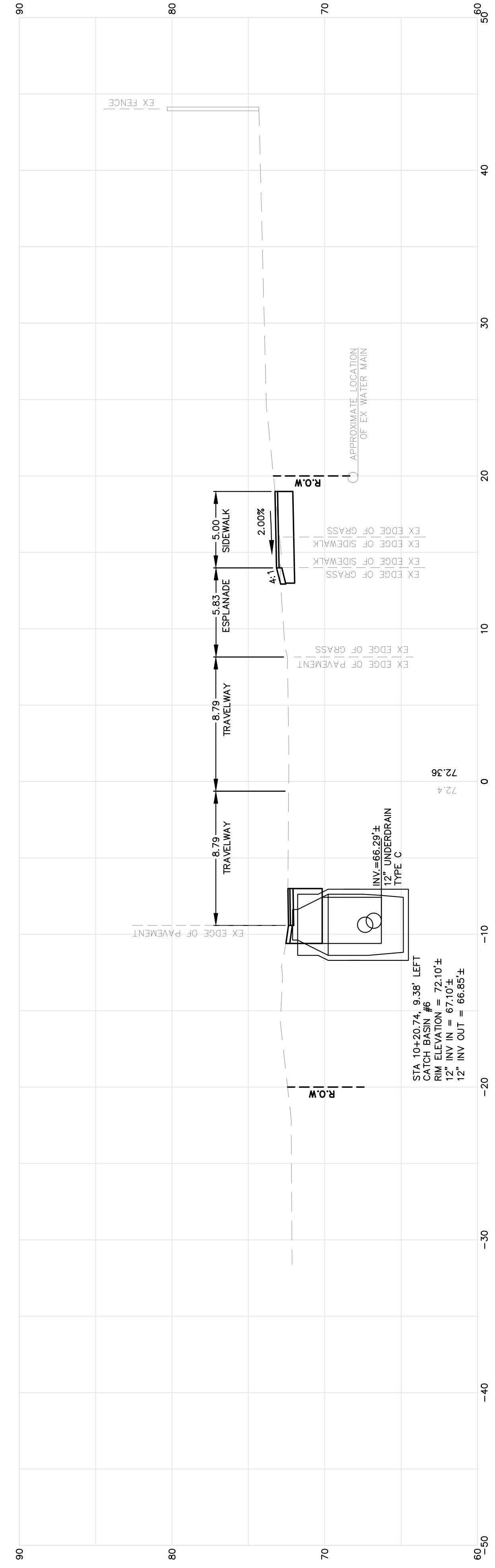
WIN  
024357.00

DATE  
03/31/2021

HIGHWAY PLANS



SHEET NUMBER		10	
OF 15			
CITY OF EASTPORT HIGH STREET SIDEWALK			
5+22.76 CROSS SECTIONS 6+50.00			
PROJ. MANAGER	BY	DATE	SIGNATURE
DESIGN-DETAILED	-	-	-
CHECKED-REVIEWED	-	-	-
DESIGN-DETAILED	-	-	-
DESIGN-DETAILED	-	-	-
REVISIONS 1	-	-	-
REVISIONS 2	-	-	-
REVISIONS 3	-	-	-
REVISIONS 4	-	-	-
FIELD CHANGES	-	-	-
STATE OF MAINE DEPARTMENT OF TRANSPORTATION		DATE 03/31/2021	
HIGHWAY PLANS		WIN 024357.00	



SHEET NUMBER  
 12  
 OF 15

CITY OF EASTPORT  
 HIGH STREET SIDEWALK

8+50.00 CROSS SECTIONS 10+00.00

PROJ. MANAGER	BY	DATE
DESIGN-DETAILED	-	-
CHECKED-REVIEWED	-	-
DESIGN-DETAILED	-	-
DESIGN-DETAILED	-	-
REVISIONS 1	-	-
REVISIONS 2	-	-
REVISIONS 3	-	-
REVISIONS 4	-	-
FIELD CHANGES	-	-

SIGNATURE	P.E. NUMBER	DATE
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

STATE OF MAINE  
 DEPARTMENT OF TRANSPORTATION

WIN  
 024357.00

HIGHWAY PLANS